

| MAIN DATA                           |          | CP313            |
|-------------------------------------|----------|------------------|
| Sales Unit Name                     |          | E2               |
| Elevator system / Technical cluster |          | ES1 / 1.2.1      |
| Elevator category                   |          | Person Elevator  |
| Rated load [kg]                     | GQ       | 675              |
| Number of passengers                | ZQG      | 9                |
| Rated speed of car [m/s]            | VKN      | 1.60             |
| Travel height [m]                   | HQ       | 11.20            |
| Roping                              | KZU      | 2                |
| Number of stops                     | ZE       | 5                |
| Number of LD front per elevator     | ZEZ1     | 5                |
| Number of LD rear per elevator      | ZEZ2     | 0                |
| Control type                        |          | Scalable Control |
| Control system                      |          | KA               |
| Number of elevators in group        | ZAG      | 1                |
| Regulation code                     |          | EN 81-20:2014    |
| Handicapped code                    |          | EN 81-70:2018    |
| Building tolerance                  |          | -20/+20 mm       |
| Vandal resistance category          |          | No vandalism     |
| Fire code                           |          | No               |
| Seismic code / Seismic category     |          | No               |
| Car width x Car depth               | BKxTK    | 1200x1400        |
| Clear car width                     | BK_Clear | 1200             |

| DRIVE DATA                           |                |
|--------------------------------------|----------------|
| Machine type                         | PMB125-C08-528 |
| Traction sheave diameter [mm]        | DD 87          |
| Balancing of load [%]                | KG 50          |
| Number of suspension media           | ZZ 2           |
| Car Total length of 1 susp.media [m] | 33             |
| Width of suspension media [mm]       | BZ 30          |
| Inverter type                        | VF VAF023 480  |

| CAR DATA                                |             |
|---|-------------|
| Car type                                | CA PK 44    |
| Car sling type                          | -           |
| Car door type                           | DO VAR 15   |
| Car guideshoes type                     | B029D       |
| Car safety gear type                    | SA_GED_10   |
| Weight of car [kg]                      | GK 497.38   |
| Masses acting upon car safety gear [kg] | GKU 1175    |
| Car weight during installation [kg]     | GK_INEX 252 |

| LANDING DOOR DATA           |                           |
|-----------------------------|---------------------------|
| Landing door type           | DO WIV EU (Wittur Evo EU) |
| Fire rating of landing door | EN 81-58 E120             |

| COUNTERWEIGHT DATA                     |                 |
|--|-----------------|
| CWT type                               | GG41-1002-106-B |
| CWT guideshoes type                    | I7              |
| CWT safety gear type                   | Not ordered     |
| Weight of CWT [kg]                     | GG 835          |
| Masses acting upon CWT safety gear[kg] | GGU -           |

| MECHANICAL EQUIPMENT                      |                           |
|---|---------------------------|
| Compensating media type                   | Not ordered               |
| Number compensating media                 | ZU -                      |
| Weight of one comp. media per m [kg]      | GUM1 -                    |
| Car Ov. governor rope diameter [mm]       | 6                         |
| Car Ov. governor rope type                | Seale 6x19S SFC 1770 B sZ |
| Car guide rail type                       | T75-3/B                   |
| Counterweight guide rail type             | H50                       |
| Car buffer type                           | LSB16                     |
| CWT buffer type                           | LSB16                     |
| Car overspeed governor type               | GBP201                    |
| Car Total length of Ov. Governor Rope [m] | 32                        |
| Car tension device type                   | 201CB                     |
| CWT overspeed governor type               | Not ordered               |
| CWT Total length of Ov. Gov. rope [m]     | LCR -                     |
| CWT tension device type                   | Not ordered               |

|   |                      |
|---|----------------------|
| Subsystem of Unintended Car Movement Protection |                      |
| Detection Means                                 | AC_GSI_100_2FS       |
| Certificate number                              | 01/208/4A/6136.00/19 |
| Stopping Means                                  | 2X100 Nm             |
| Certificate number                              | NL19-400-1002-051-02 |

| ELECTRICAL PARAMETERS                              |                           |                                |
|--|---------------------------|--------------------------------|
| Operating temperature range [°C]                   | T_Operation_Range         | +5/+40                         |
| Humidity [%]                                       | Humidity_Range_Electrical | max 60% at 40°C or 85% at 25°C |
| Altitude above sea level [m]                       | HAM                       | ≤2000                          |
| Cable routing type when MMR/MR                     | MR_Cable_Routing          |                                |
| Number of starts per hour max.                     | ZKH_max                   | 180                            |
| Main power supply                                  | Supply_Power_Net_Type     | TN-S                           |
| Neutral wire                                       | Neutral_Wire              | Yes                            |
| Rated mains [V] / Mains voltage tolerance [%]      | UN / UN_Tol_Range         | 400 / -15/+10                  |
| Mains voltage asymmetry range [%]                  | UN_Phase_Asymmetry_Range  | 5/+5                           |
| Mains current during constant speed [A]            | INN                       | 13.92                          |
| Mains current during acceleration [A]              | INA                       | 16.60                          |
| Mains frequency [Hz] / Tolerance [%]               | FN / FN_Tol_Range         | 50 / -5/+5                     |
| Main switch  | JH_Variant                | MCB_C16A                       |
| Max current of overcurrent prot. dev. building [A] | SIH_Size                  | Not relevant                   |
| Cable cross section at JH min / max [mm²]          | ANN_JH_min/_max           | 1 / 25                         |
| Failure current maximum [mA]                       | I_Delta_N_max             | 300                            |
| Short circuit current rating max. [kA]             | SCCR_max                  | 6                              |
| Max total harmonic distortion mains current [%]    | THDI_max                  | 37                             |
| Surge protection voltage max [kV]                  | USP_Max                   | -                              |
| RCD type failure current switch on bldg. side      | JFIH_Opt                  | No                             |
| Maximum regenerative power [W]                     | PNAG                      | 4486.00                        |
| Mains line impedance max [mOhm]                    | ZFN_max                   | 300                            |
| Power factor minimum                               | PS_Ratio_min              | 0.92                           |
| Mains distortion Cos Phi                           | Cos_Phi_JH                | 0.99                           |
| Mains apparent pow. const. speed / end accel. [VA] | SNN / SNA                 | 9642.22168 / 11497.669154      |
| Mains voltage lighting [V] / Tolerance [%]         | UNL / UNL_Tol_Range       | 230 / -15/+10                  |
| Lighting current [A]                               | INL                       | 10                             |
| Main switch lighting                               | JHL_Type                  | RCBO C10A 30mA Type A          |
| Cable cross section at JHL min / max [mm²]         | ANN_JHL_min/_max          | 1 / 16                         |
| Main switch lighting hoistway                      | SIBS_Type                 | RCBO C6A 30mA Type A           |
| Hoistway lighting current [A]                      | I_SIBS                    | 6.00                           |
| Cable cross section for SIBS min / max [mm²]       | ANN_SIBS_min/_max         | 1.00 / 16.00                   |
| Automatic evacuation system Attention: power!      | AES_Opt                   | No                             |
| Max. number of automatic evacuation trips in a row | Z_Evac                    | 0                              |

| CAR DECORATION                         |                                |
|--|--------------------------------|
| Car front finish                       | St.steel AISI441 brushed       |
| Door finish                            | St.steel AISI441 brushed       |
| Side walls material                    | PB Pyroex laminate CPL)        |
| Side walls finish                      | NCS S 3030-R90B                |
| Rear wall material                     | PB Pyroex laminate CPL)        |
| Rear wall finish                       | NCS S 3030-R90B                |
| Car skirting finish                    | Aluminum anodized gray         |
| Car skirting alignment                 | Flush                          |
| Car skirting shape                     | Straight                       |
| Floor material                         | Rubber                         |
| Floor finish                           | Speckled rubber dark gray      |
| Car decoration line                    | Times Sq.                      |
| Ceiling type                           | Line                           |
| Ceiling decoration                     | St.steel AISI441 brushed       |
| Mirror left                            | Not ordered                    |
| Mirror rear                            | Half height par. width; center |
| Mirror right                           | Not ordered                    |
| Rear wall glass type                   | Not ordered                    |
| Side wall glass type                   | Not ordered                    |
| Handrail finish                        | St.steel AISI304 brushed       |
| Handrail left                          | No                             |
| Handrail right                         | Parametric                     |
| Handrail rear                          | No                             |
| Bumper Rails Design                    | -                              |
| Bumper Rails Type                      | -                              |
| Weight of car decoration GKD)          | -                              |
| Weight of custom ceiling               | -                              |
| Weight of custom floor                 | -                              |
| Weight of additional custom decoration | -                              |
| Weight of custom decoration            | -                              |

- AKV= Car area
- BS= width shaft
- BT= width door
- BK= width car
- BKS= width car guide
- BGS= width cwt guide
- BG= width cwt
- COP= Car operation panel
- HT= height door
- HE= height floor
- HQ= height travel
- HS= height shaft
- HSG= height shaft pit
- HSK= height shaft headroom
- HF= Distances between guide rail fastening brackets
- HK= Car height
- HKC= Inside car height
- HKZ= Height car flooring
- HGP= Distance from counterweight to buffer
- HKP= Distance from buffer plate on car to buffer or plinth, with car at lowest terminal
- HP= Height of buffers, fully extended
- HPH= Rounded up total of buffer stroke and rubber stroke:
- HSS1= Height of plinth underneath car
- HSS2= Height of plinth underneath counterweight
- LFGK= Length of cwt rail end from top floor
- LFKK= Length of car rail end from top floor
- LOP= Landing operation panel
- SG= guide cwt bracket
- SF= guide car bracket
- SKU= lift overtravel bottom)
- SKO= lift overtravel top)
- SKS= Jump distance of car
- TS= depth shaft
- TK= depth car
- TG= depth cwt
- TKF= Distance between edge of car sill and guide rail axis
- TSW= Distance from hoistway front wall to landing door sill
- TKSW= Distance from hoistway front wall to center line of car guides



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|                             |  |                               |  |
|-----------------------------|--|-------------------------------|--|
| <b>General Information:</b> |  | Product Line:<br><b>S3000</b> |  |
| Building                    | <b>HED 675 kg</b>  |                               |  |
| Address                     | Gjótuhraun 4 - 220 Hafnarfjörður                               |                               |  |
| Client                      | Hedinn Schindler Lyftur H.F - Gjótuhraun 4 - 220 Hafnarfjörður |                               |  |



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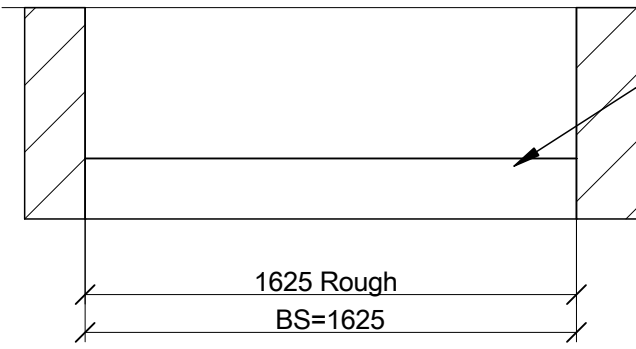
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A3

# Door Detail 1:25

Entrance: 4 Control box - LDU



Electrical supply line: local supply)  
 - 400 V Main power switch  
 - 230 V Hoistway lighting  
 Cable reserve 2 m min.

### Hoistway loads [N]

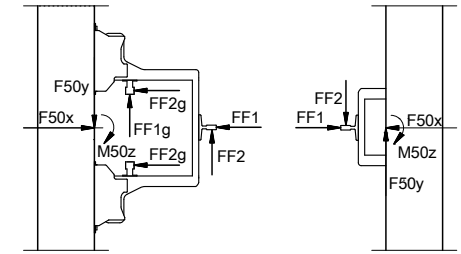
|             |         |                |
|-------------|---------|----------------|
| F9 = 23003  | F17 = - | F50x_T = 776   |
| F10 = 32808 | F41 = - | F50y_T = 630   |
| F11 = 34155 | F42 = - | M50z_T = 144   |
| F12 = 17783 | F43 = - | F50x_PH = 1358 |
| F13 = 6340  | F44 = - | F50y_PH = 1095 |
| F14 = 3878  | F45 = - | M50z_PH = 250  |

Load F11 & F12 only occur in case of operation of the safety gear.  
 Loads F9 & F10 in case either car or counterweight lands on the buffers.

PH = Pit Head T = Travel

### Guide shoe forces (max. dynamic) [N]

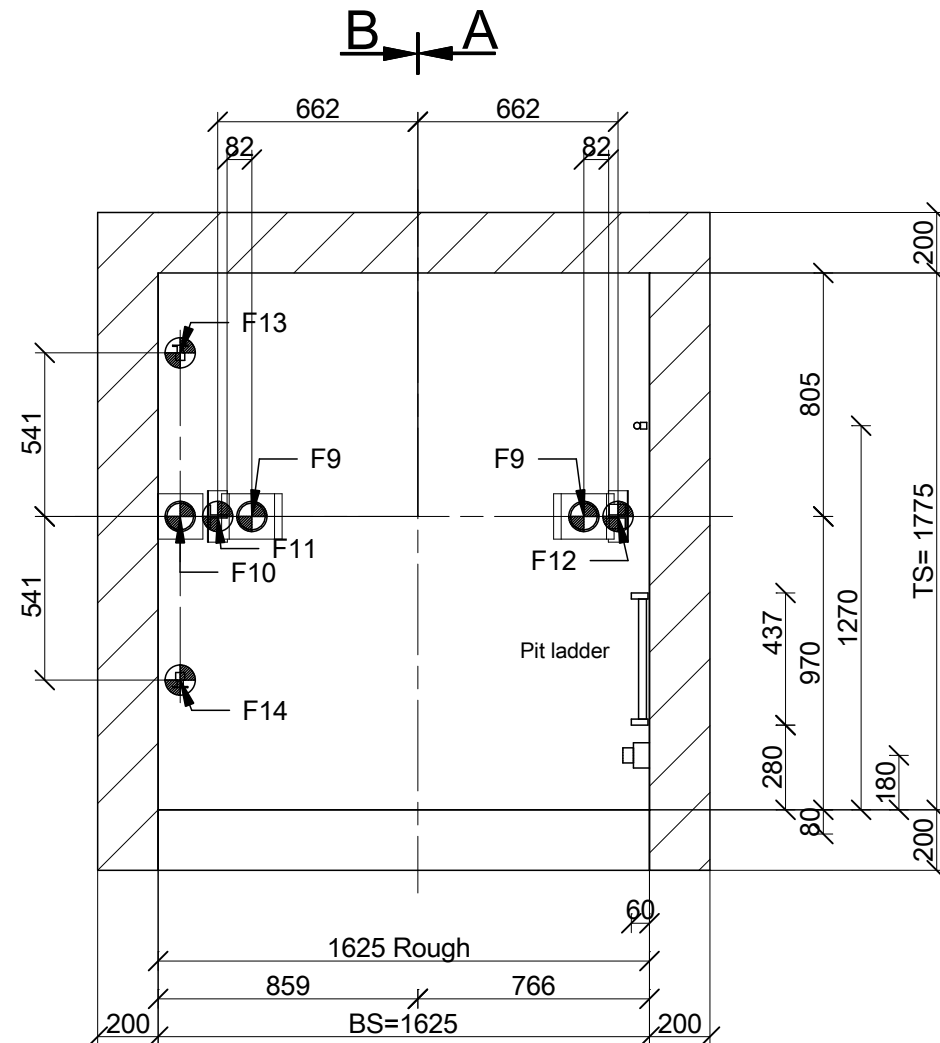
Car  
 FF1 = -  
 FF2 = -  
 Counterweight:  
 FF1g = -  
 FF2g = -



### Sound Insulation

The well construction must be adequate to comply with current noise requirements and relevant regulations for adjacent rooms.

# Well 1:25



Entrance: 0, 1, 2, 3, 4

Provide a horizontal datum for sill threshold adjacent to doorway openings at each level to enable installation of doorjamb.

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**BUILDER'S WORKPLAN** Product Line: **S3000**

Building **HED 675 kg**  
 Address **Gjótuhraun 4 - 220 Hafnarfjörður**  
 Client **Hedinn Schindler Lyftur H.F - Gjótuhraun 4 - 220 Hafnarfjörður**



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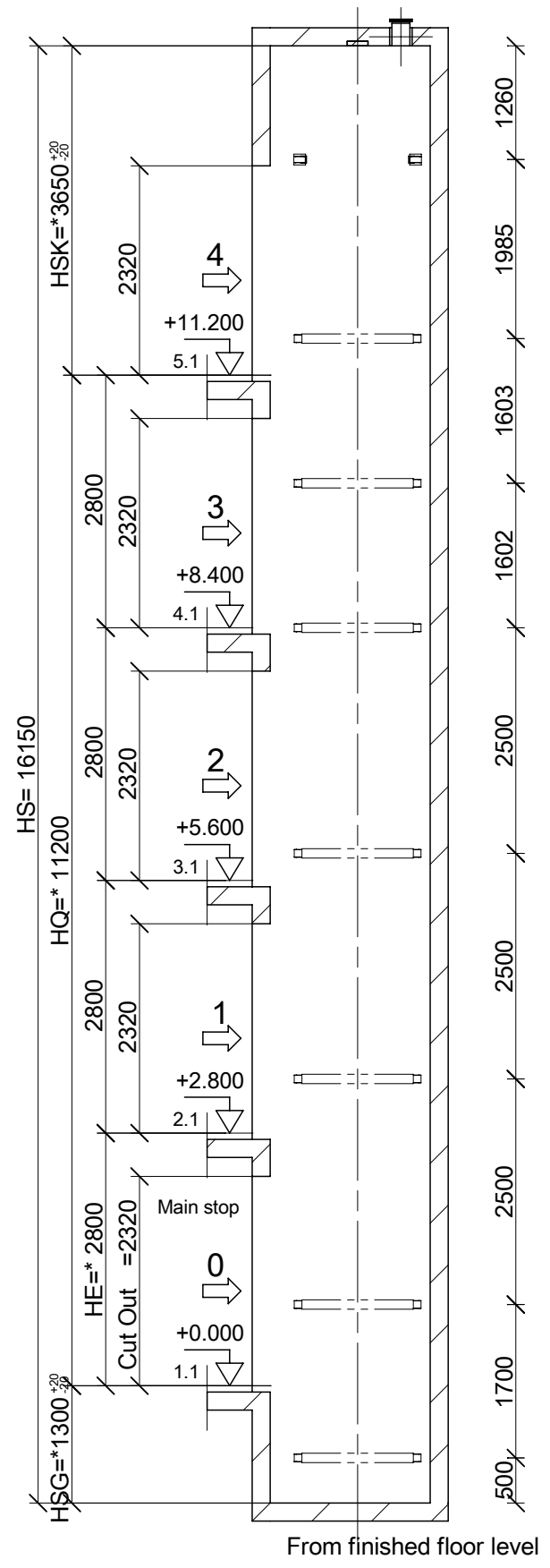
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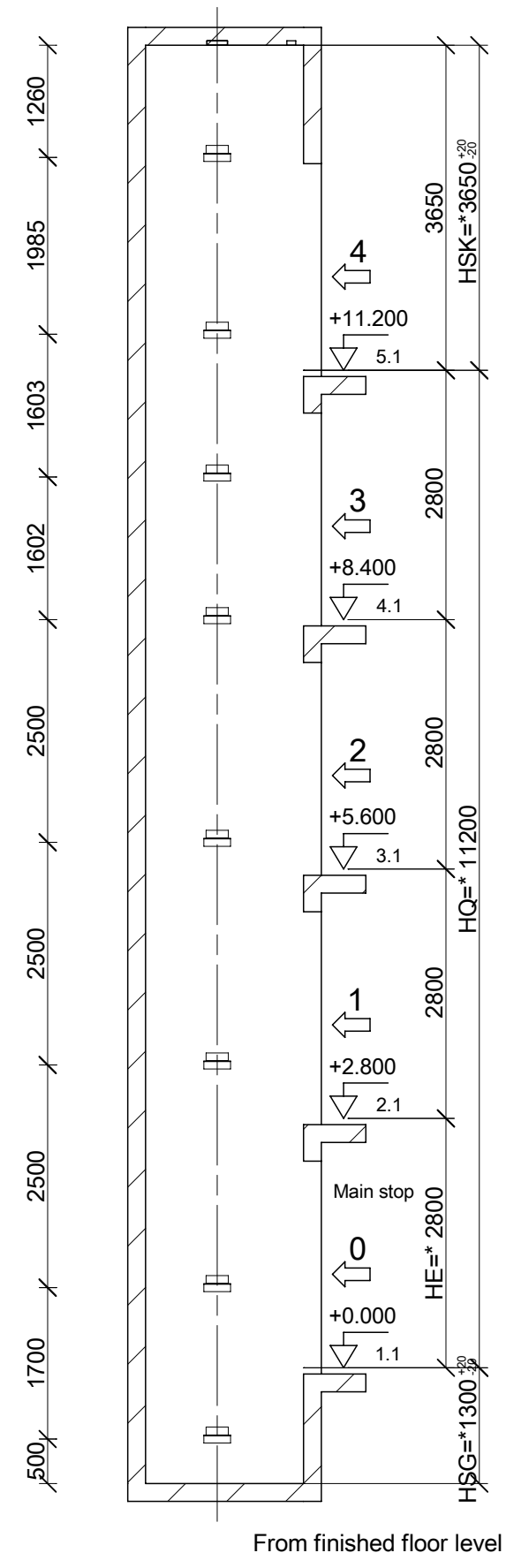
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### Section A-A 1:75



### Section B-B 1:75



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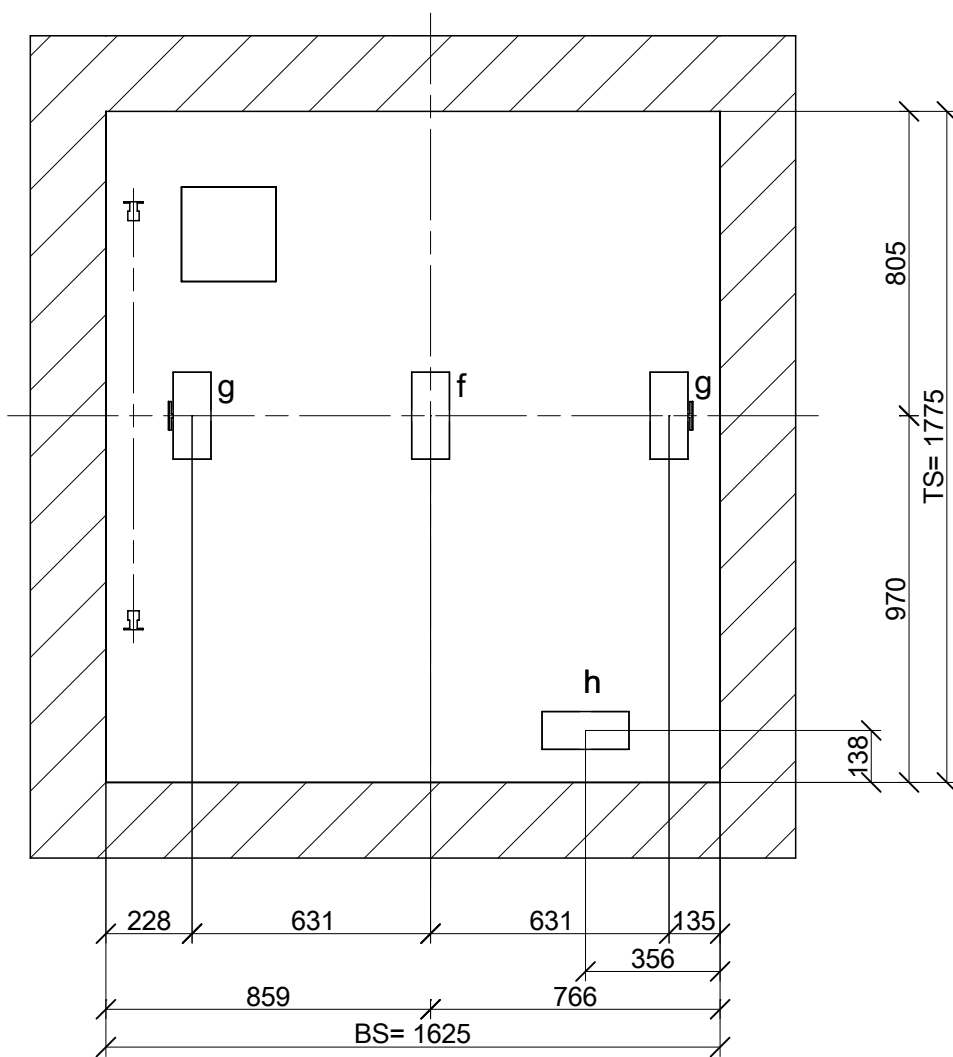
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## Well Head 1:20



### Inlay Parts:

| Description | Description Type  | Quantity |
|-------------|---|----------|
| Ceiling f   | Hitch box HBL<br>Black color, Double loop, SWL/WLL 1400 kg each loop) | 1        |
| Ceiling g   | Hitch box HBLE<br>Grey color, Single loop, 1100 kg)                   | 2        |
| Ceiling h   | Optional HBS<br>Red color, Single loop, 1200 kg Optional)             | 1        |

SWL/WLL = Hitch point work load limit  
 - All inlay parts named with SWL/WLL has to be tested with  $\geq 2 \times \text{SWL/WLL}$   
 - For all inlay parts named with SWL/WLL builder must calculated the ceiling for  $\geq 4 \times \text{SWL/WLL}$   
 All inlay parts have to be labeled at the lifting point!

**Well enclosure**  
 The structure of the well, machinery and pulley rooms shall conform to National Building Regulations and be able to support at least the loads specified into this drawing. EN 81-20/50 5.2.1.8.1)  
 Hosting facilities in top of the well must be provided according to the specification on this drawing. Airborne noise generated by the drive unit is 62dB(A(Leq), 65dB(A(impulse)). The top part of the well must be properly designed and constructed in a way to assure the fulfillment of the National Noise Regulations into the adjacent rooms.  
 All measurements are finished masonry i.e. complete with plaster.  
 Indicated scales refer to the original drawing size.  
 Final floor levels must be clearly defined and marked prior to the start of the lift installation. Well openings must be protected according to the local regulations to prevent people falling down by accident. In case of absence of relevant local regulations, we recommended to protect such openings according to the minimum requirements defined

General well construction conditions must be in accordance with EN 81-20/50 :  
 Well, machinery and pulley rooms EN 81-20/50 5.2):  
 EN 81-20/50 5.2.1.2.1):  
 The well, machine and pulley rooms shall not be used for purpose other than lifts  
 They shall not contain ducts, cables or devices other than for the lifts  
 EN 81-20/50 5.2.1.9):  
 Surfaces of walls, floors and ceiling of wells, machine and pulley rooms shall be in durable material not favoring the creation of dust e. g. concrete, brick or blockwork.  
 The surface of the floor where a person needs to work or to move between working areas shall be of non-slip material.  
 The floor of working areas shall be approximately level, except for any buffer and guide rail bases and water drainage devices.  
 After the building-in of guide rail fixings, buffers, any grids, etc.. the pit shall be impervious to infiltration of water.  
 EN 81-20/50 5.2.5.4)  
 If accessible spaces do exist below the well, the base of the pit shall be designed for an imposed load of at least 5000 N/m<sup>2</sup>, and the counterweight or the balancing weight shall be equipped with safety gear.  
 EN 81-20/50 5.2.1.3)  
 The well, machinery spaces and pulley rooms shall not be used to provide ventilation of rooms other than those belonging to the lift.  
 Ventilation shall be such that the motors and equipment, as well as electric cables, etc.. are protected from dust, harmful fumes and humidity.  
 EN 81-20/50 5.2.1.4.1)  
 The well shall be provided with permanently installed electric lighting, giving the following intensity of illumination, even when all doors are closed, at any position of the car throughout its travel in the well:  
 a) At least 50 lux, 1,0 m above the car roof within its vertical protection  
 b) At least 50 lux, 1,0 m above the pit floor everywhere a person can stand, work and/or move between the working areas.  
 c) At least 20 lux outside of the locations defined in a) and b), excluding shadows created by car or components.  
 Lighting elements shall be protected against mechanical damage. Air conditioning or forced ventilation in well if needed) must be designed and provided by others.  
 Mains supply  
 Length of supply cable for controller cabinet LDU) has to stand out min. 1m over the rough floor  
 If the feeding network is type TT then RCD is required to protect the elevator power and lightning feeding wiring.  
 If a Residual Current Device (RCD) is used to protect the elevator power supply, it shall be of the type B.  
 If a Residual Current Device (RCD) is used to protect the elevator light supply, it shall be of the type A with selective "S" characteristic.  
 One RCD shall protect only one elevator power or lighting supply.  
 PE conductor: The touch current in the protective earthing conductor exceeds 3,5 mA a.c. or 10 mA d.c., therefore the minimum size of the protective earthing conductor shall comply with the national regulations, e.g. reinforced protective earthing conductors.  
 Example: IEC 60364-5-54 § 543.7 identifies the reinforced protective conductors with either  $\geq 10\text{mm}^2$  copper conductor) or a second PE conductor with same cross-section as the PE delivered for protection against indirect contact.  
 Control cabinet LDU)  
 The control cabinet must be located in an area which is suitably protected against weather conditions such as rain, wind and temperatures below +5°C and above +40°C.  
 The building shall be provide at least 200 lux intensity of lighting in front of the opened control cabinet.  
 The control cabinet shall not be located in areas where interference with public can be expected leading to dangerous situations.  
 EN 81-20/50 5.2.6.3.2.1)  
 There shall be provided at least a clear height of 2,10 m at working areas, and:  
 a) a clear horizontal area in front of the control panels and cabinets. This area is defined  
 1) depth, measured from the external surface of the enclosures, at least 0,70 m  
 2) width, the greater of the following values: 0,50 m or the full width of the cabinet.  
 b) a clear horizontal area of at least 0,50 m x 0,60 m for maintenance and inspection of moving parts at points where this is necessary.

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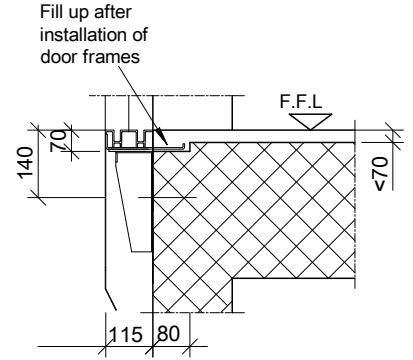
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**S3000**

Building **HED 675 kg**  
 Address **Gjótuhraun 4 - 220 Hafnarfjörður**  
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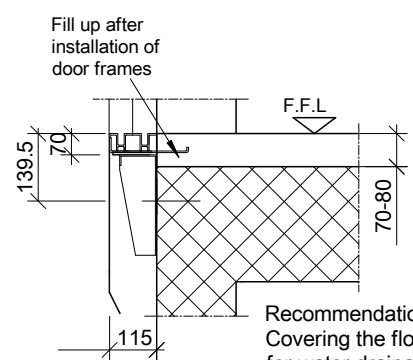
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### Door Sill Detail <70

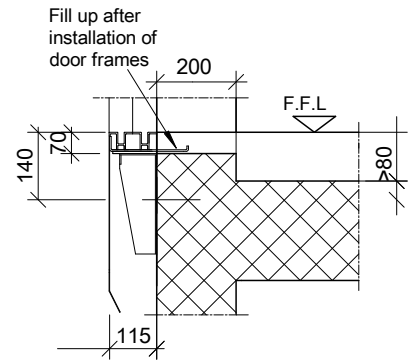


### Door Sill Detail 70-80

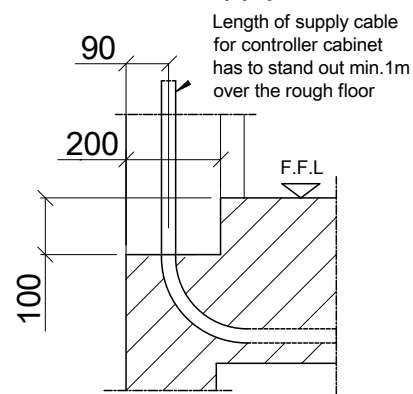


Recommendation  
Covering the floor with a  
for water drainage cleaning of  
stairs)

### Door Sill Detail >80

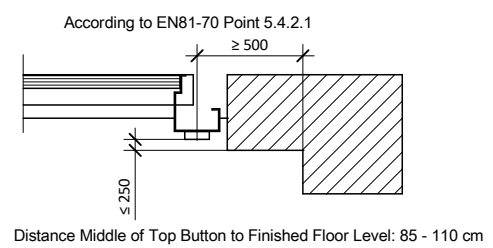


### Power Supply Cable

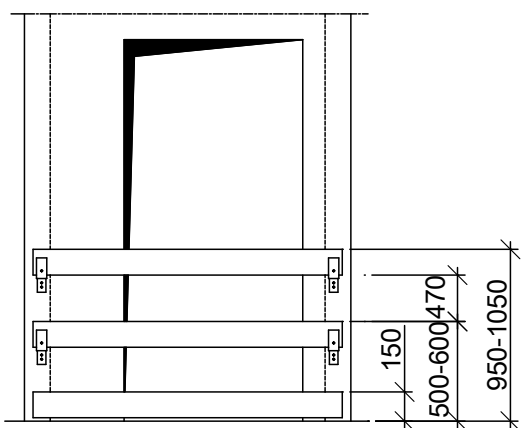


The circumferential air gap between side panels  
and wall is closed with connection profiles.  
For deviations from the vertical, the visible area  
X) between cladding and door frame changes.

### Control Devices - Requirements for Arrangement



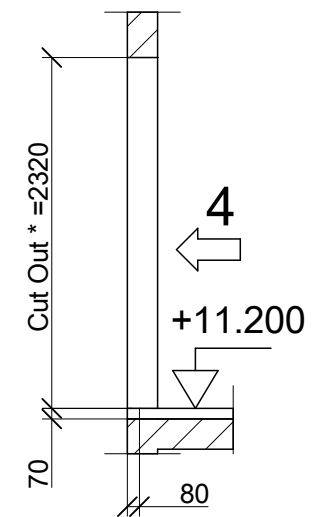
### Closing door



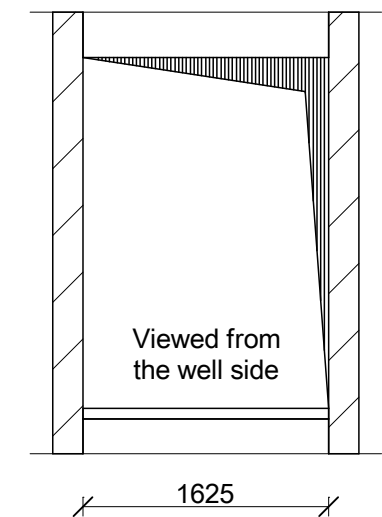
Closing door  
builders responsibility)  
The veneer plate has to be secured broad wise.  
The fence has to be easy to dismantle, and  
constructed and mounted according to the current  
regulations.

### Access side 1 1:50

Entrance: 4

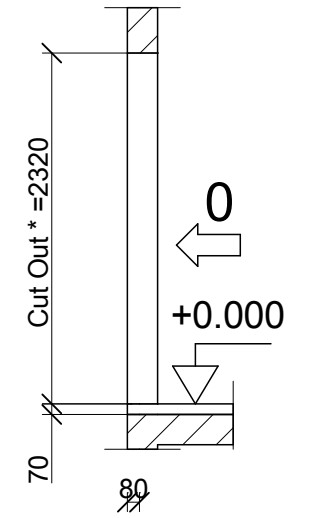


\* From finished floor level

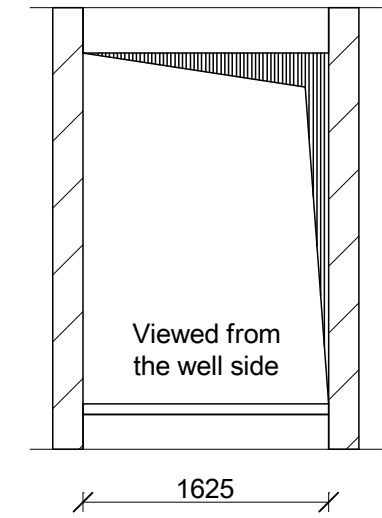


### Access side 1 1:50

Entrance: 0, 1, 2, 3



\* From finished floor level



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|                           |  |                               |  |
|---------------------------|--|-------------------------------|--|
| <b>BUILDER'S WORKPLAN</b> |  | Product Line:<br><b>S3000</b> |  |
| Building                  | <b>HED 675 kg</b>  |                               |  |
| Address                   | Gjótuhraun 4 - 220 Hafnarfjörður                               |                               |  |
| Client                    | Hedinn Schindler Lyftur H.F - Gjótuhraun 4 - 220 Hafnarfjörður |                               |  |



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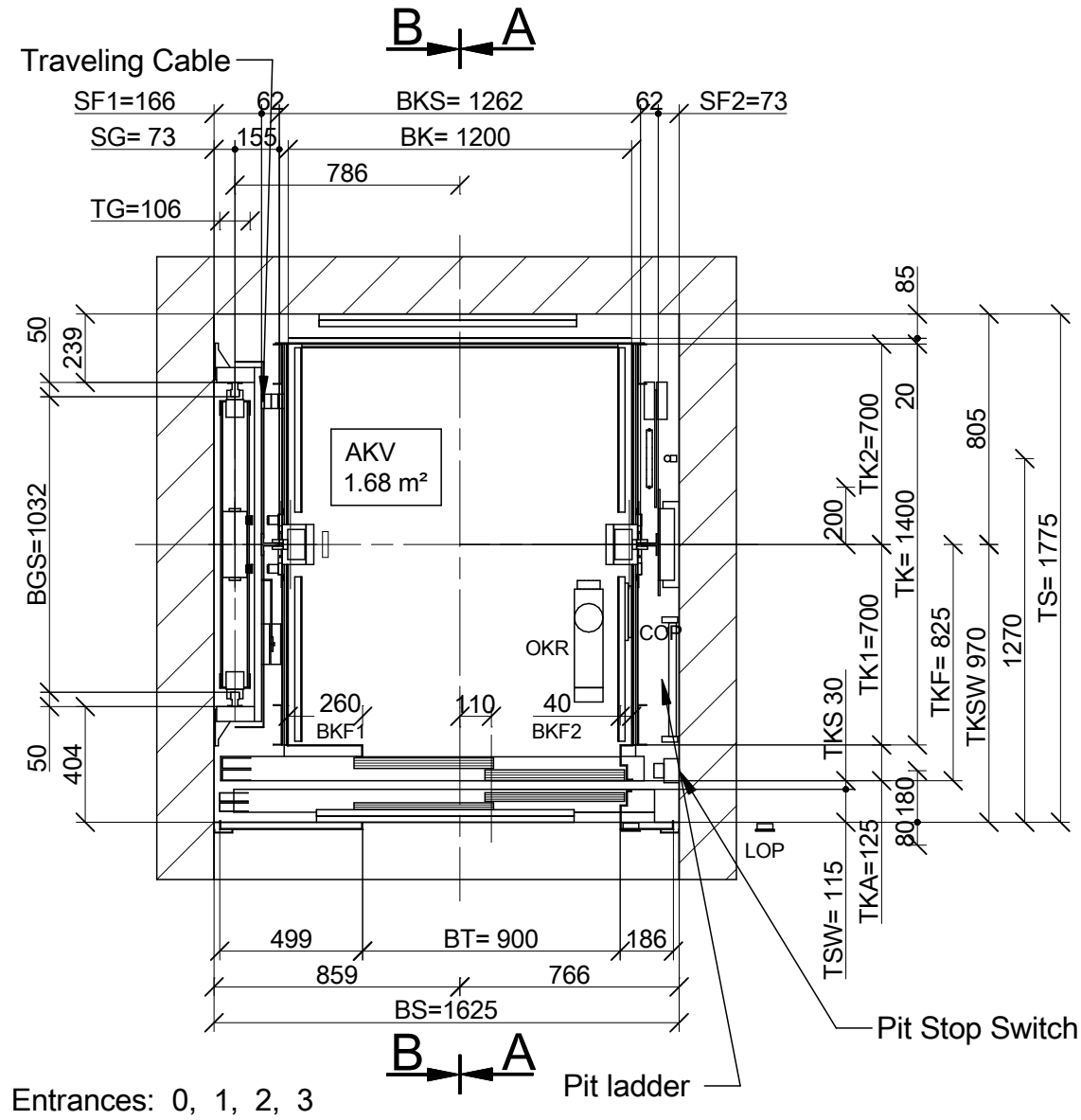
A3

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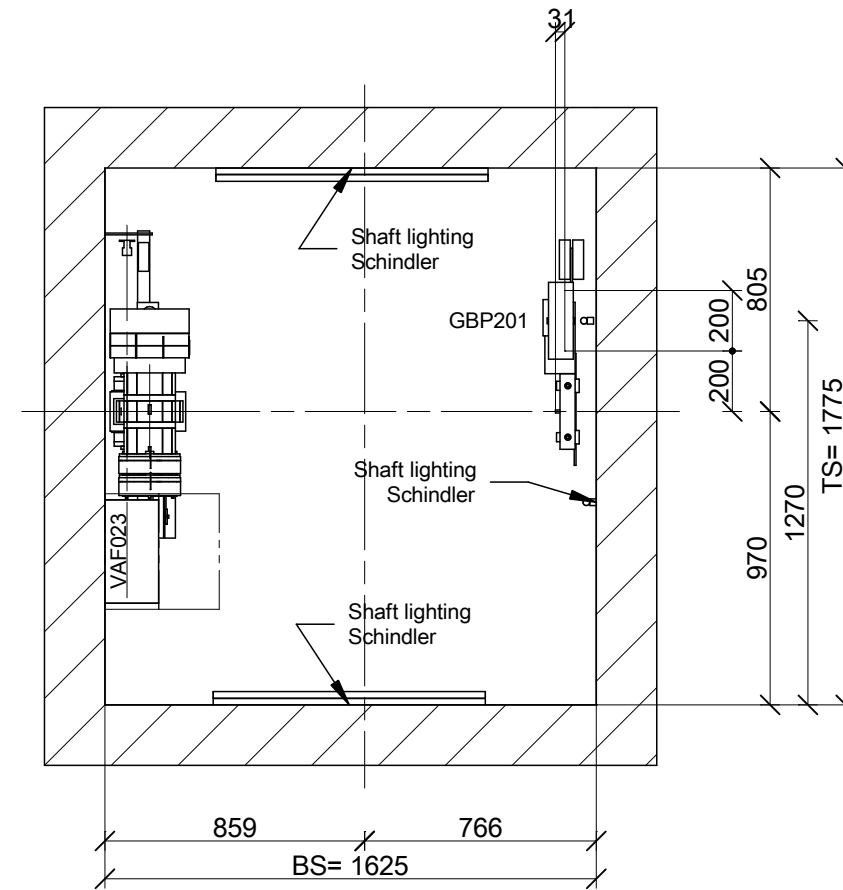
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### Well 1:25



### Well Head 1:25



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| <b>LAYOUT</b>                 |  | Product Line:<br><b>S3000</b>  |  |
| Building<br>Address<br>Client |  | <b>HED 675 kg</b><br>Gjótuhraun 4 - 220 Hafnarfjörður<br>Hedinn Schindler Lyftur H.F. - Gjótuhraun 4 - 220 Hafnarfjörður |  |

**Schindler**

Contact:

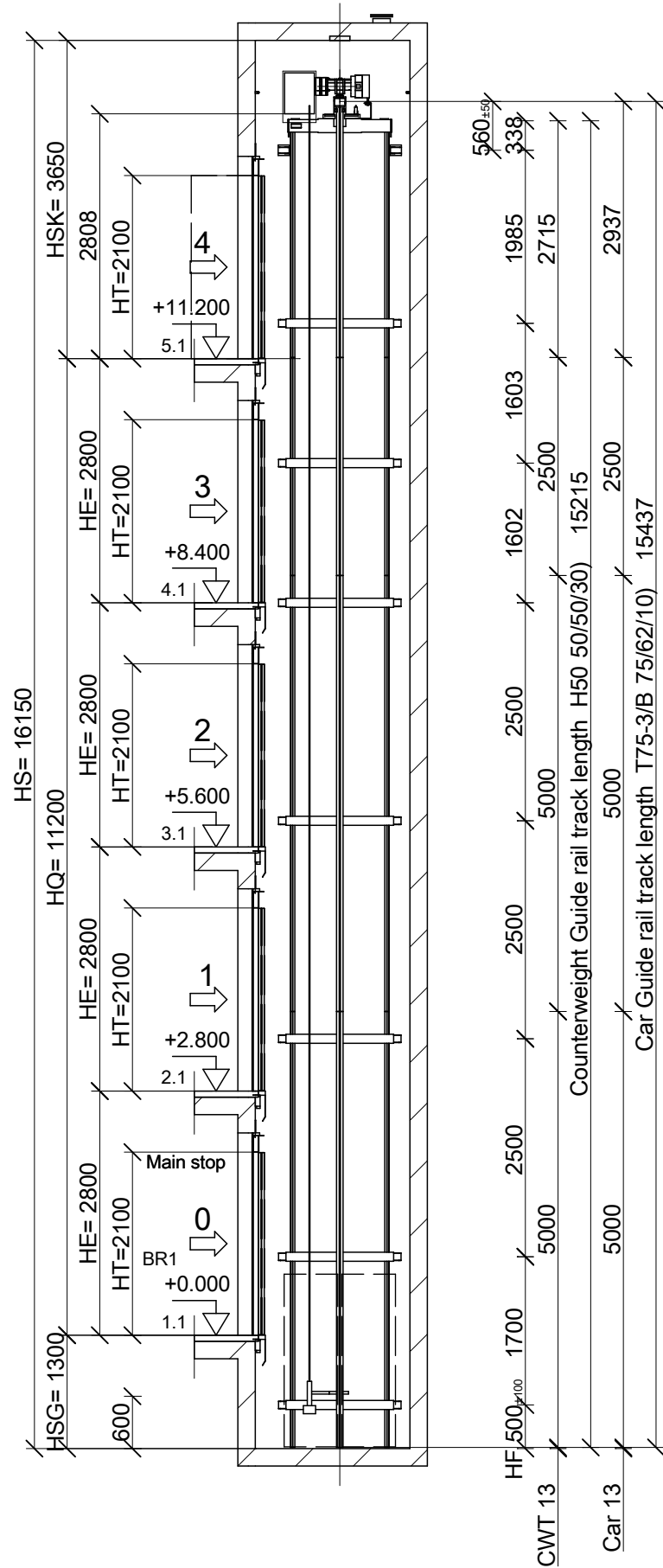
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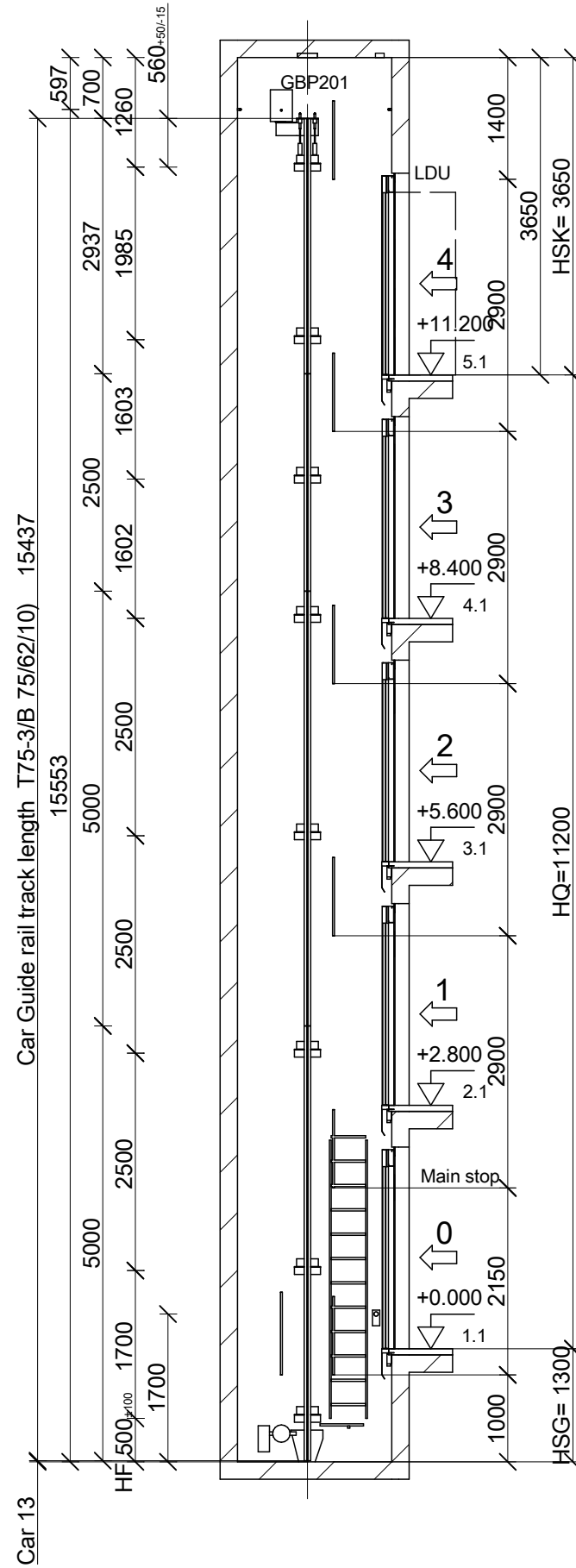
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### Section A-A 1:75



### Section B-B 1:75



### Bracket Selection

| HFmax 2500 [mm]  | Level [mm]             | Car side  | Counterweight side                       |
|------------------|------------------------|-----------|--|
| Headroom section | to 14850<br>from 10788 | 2 x Z-AL2 | 1 x L-A L 106 1<br>1 x O-A1 L 1002 106 1 |
| Travel section   | to 10787<br>from 2080  | 4 x Z-AL2 | 4 x O-A1 L 1002 106 1                    |
| Pit section      | to 2079<br>from -1300  | 2 x Z-AL2 | 2 x O-A1 L 1002 106 1                    |

\*) Brackets are marked with a sticker if they differ from bracket type in travel section.

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**LAYOUT** Product Line:  
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 Client Hedinn Schindler Lyftur H.F. - Gjótuhraun 4 - 220 Hafnarfjörður



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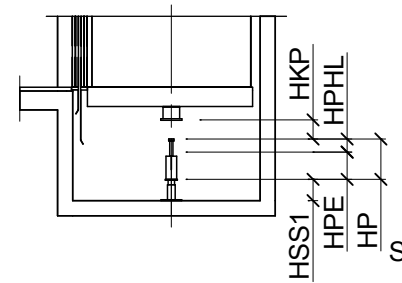
|           |                           |            |      |
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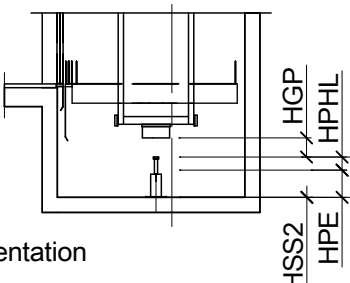
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Car position at bottom floor



Car position at top floor



Schematic presentation

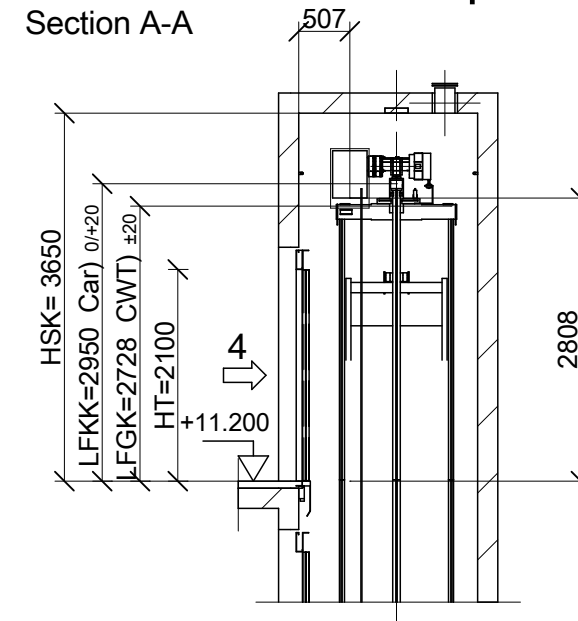
|          | Car buffer  | Counterweight buffer |
|----------|-------------|----------------------|
|          | LSB16.B     | LSB16.B              |
| HP)      | 483         | 483                  |
| HPH/HPHL | 173.5 / 175 | 173.5 / 175          |
| HKP/HGP  | 70 0/-5     | 85 0/-20             |
| HSS1/2   | 237         | 15                   |
| HPE      | 308         | 308                  |
| Quantity | 2           | 1                    |

Refuge spaces

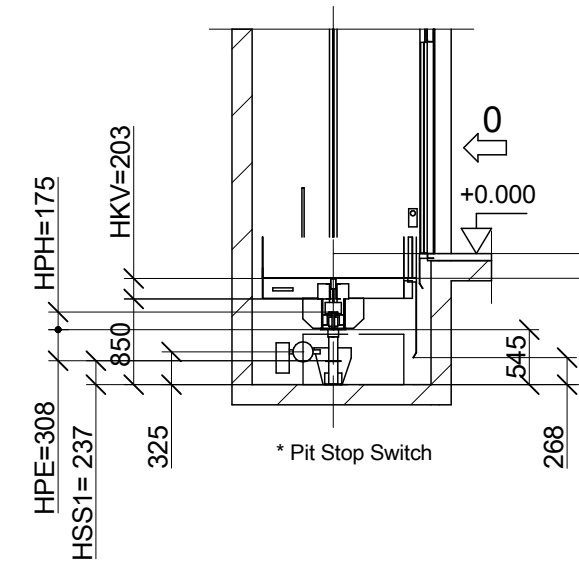
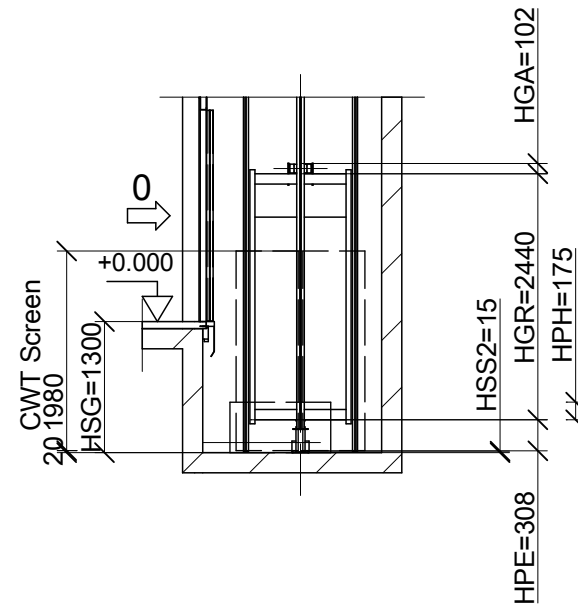
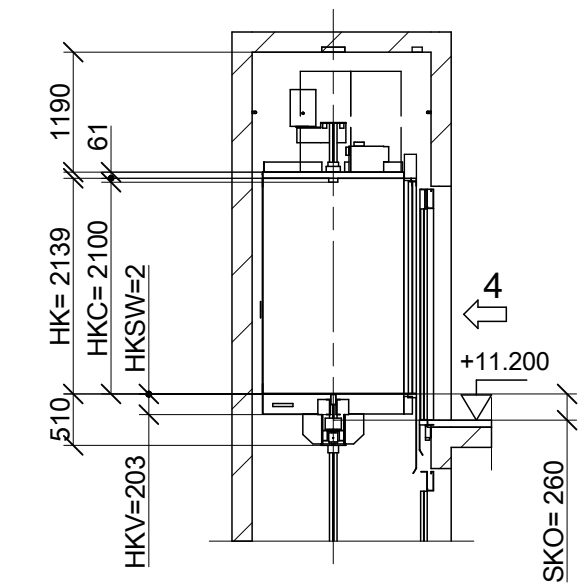
| Position and dimension                         | Label     |
|--|-----------|
| On car ceiling<br>700x500x1000<br>700x500x1000 | Crouching |
| In hoistway pit<br>700x1000x500                | Laying    |

## Wellhead and Wellpit

Section A-A



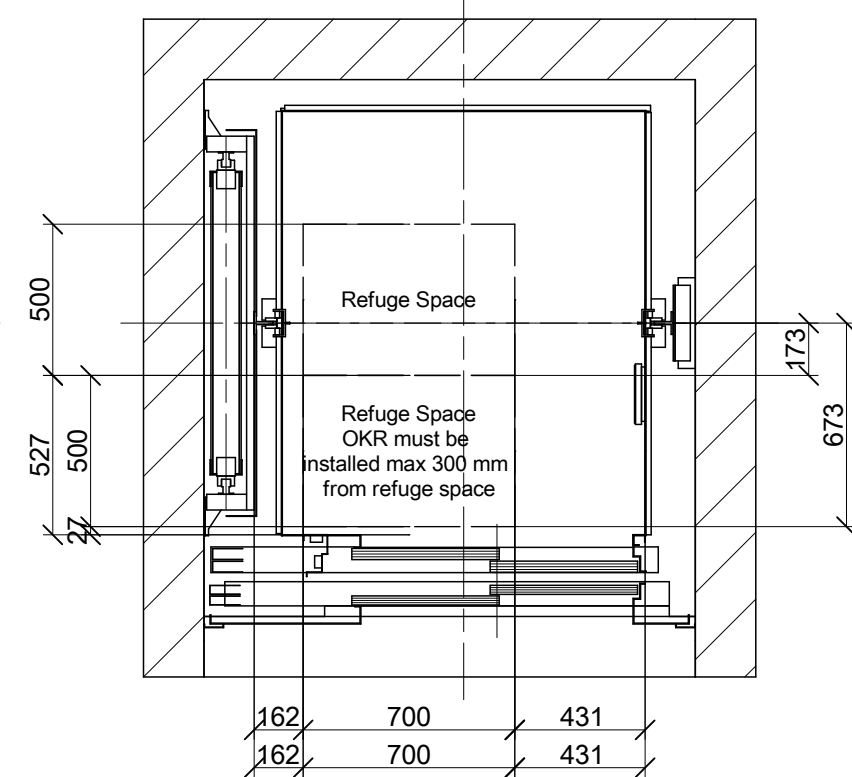
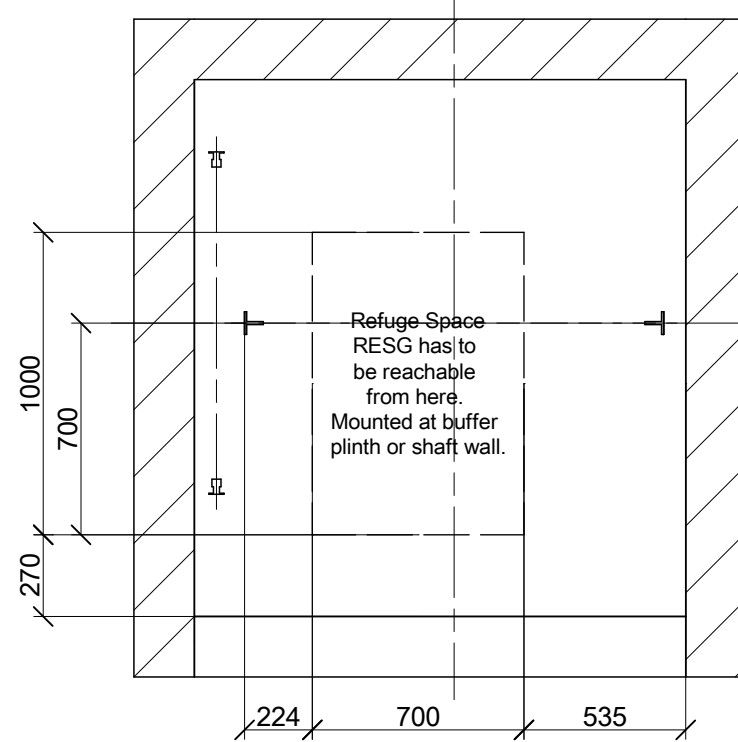
Section B-B



## Overview of refuge space situation 1:25

In hoistway pit

On car ceiling



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**LAYOUT** Product Line: **S3000**

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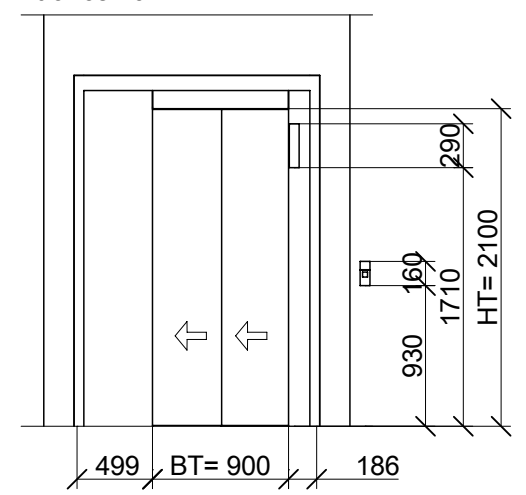
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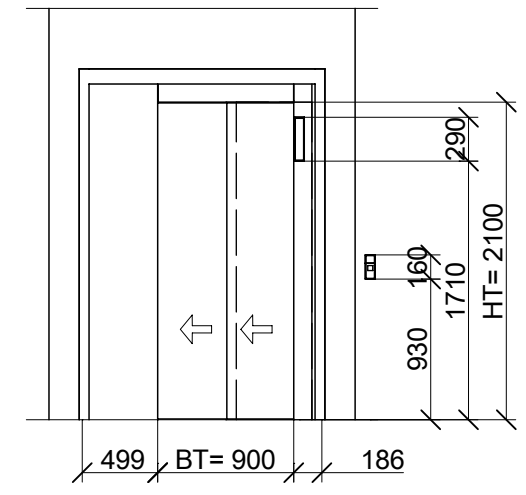
### Access side 1 1:50

Entrance: 0



### Access side 1 1:50

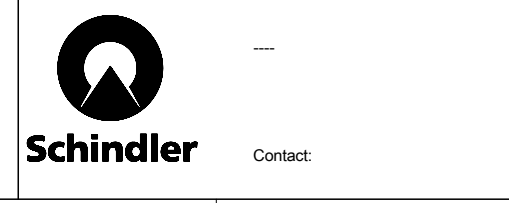
Entrance: 4 LDU / Control box



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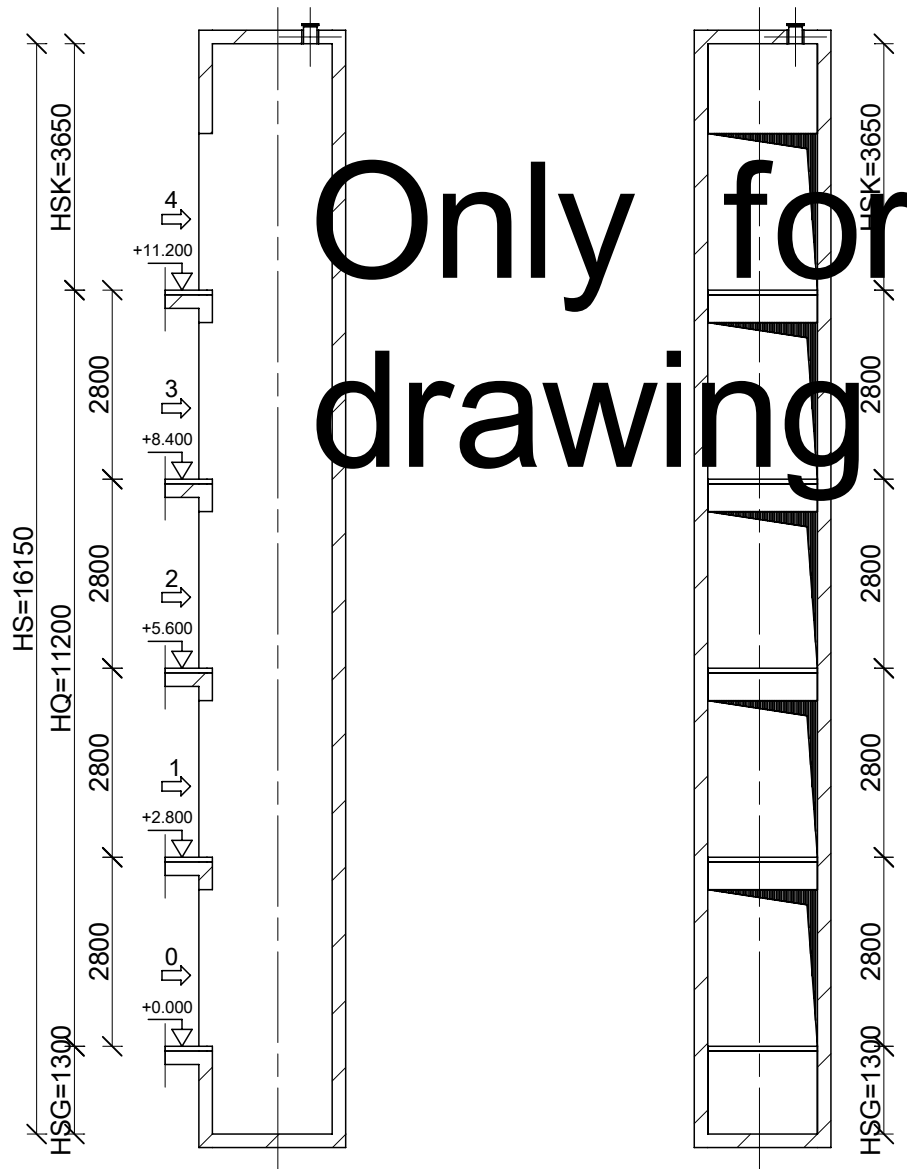
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|-----------|---------------------------|------------|------|
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**Approval - Mark**

accord

accord after modification

Date \_\_\_\_\_ Name \_\_\_\_\_

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**General Information:**

Product Line: \_\_\_\_\_

Building: **HED 675 kg**

Address: Gjótuhraun 4 - 220 Hafnarfjörður

Client: Hedinn Schindler Lyftur H.F - Gjótuhraun 4 - 220 Hafnarfjörður

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